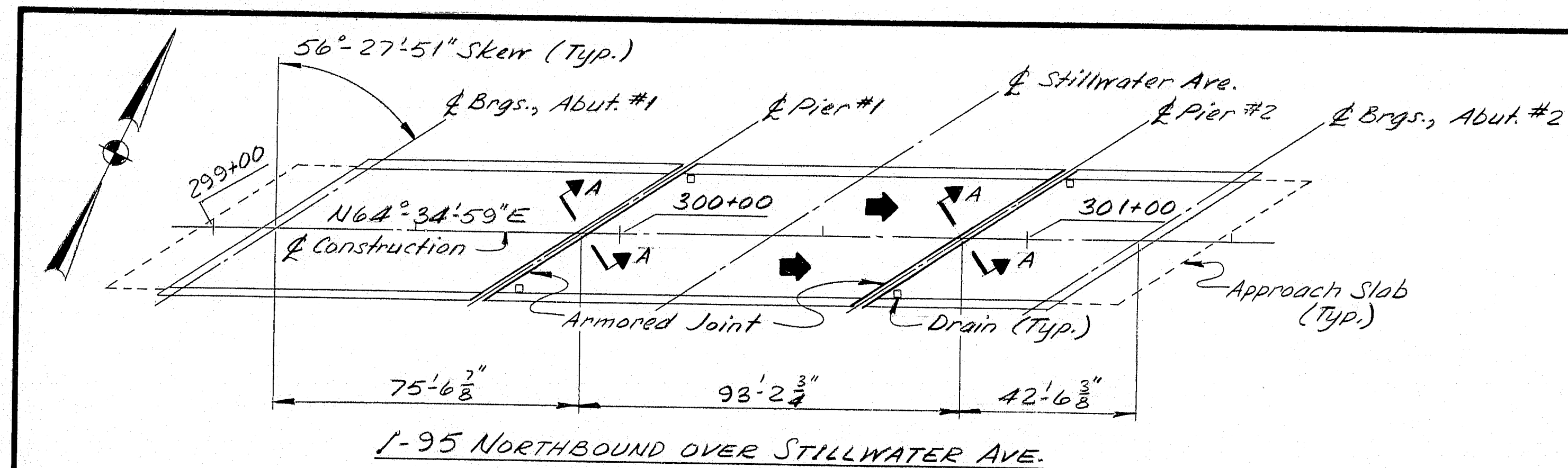
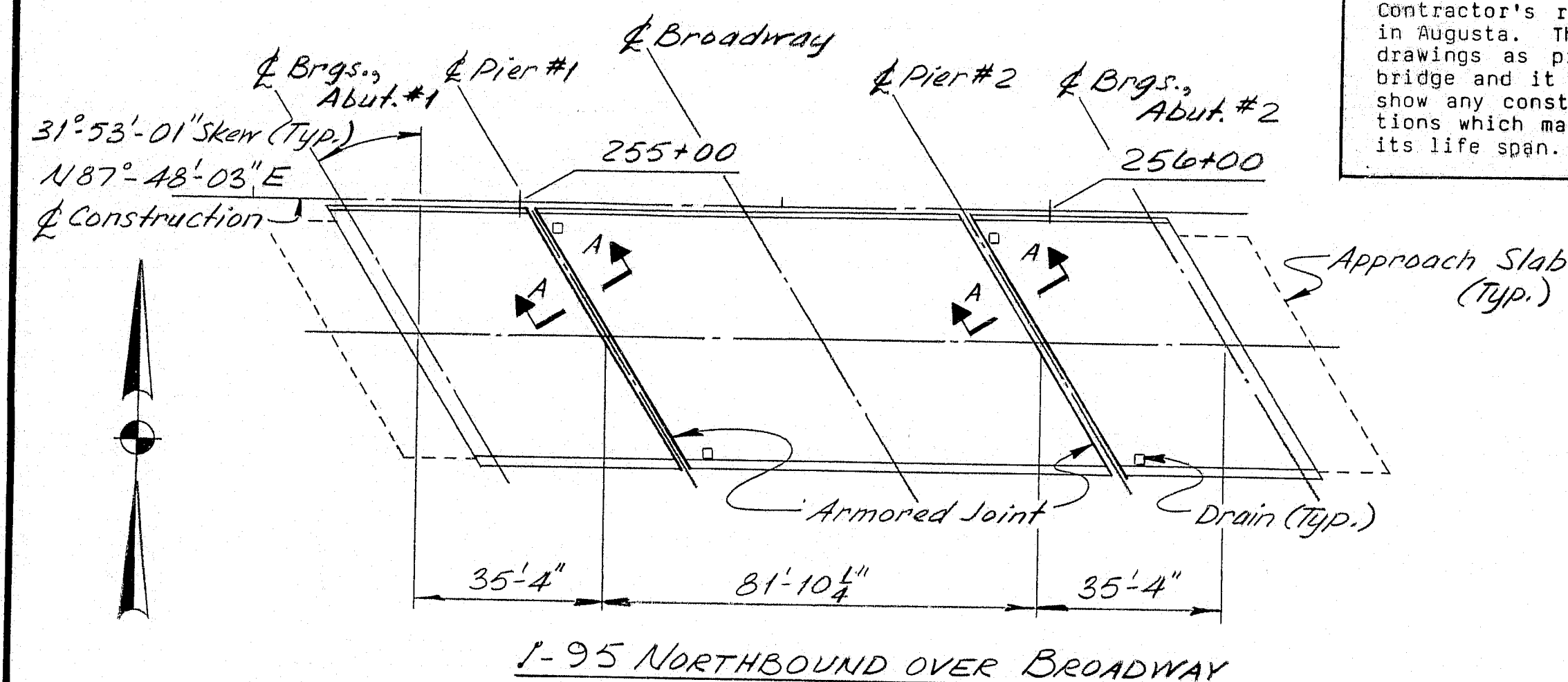


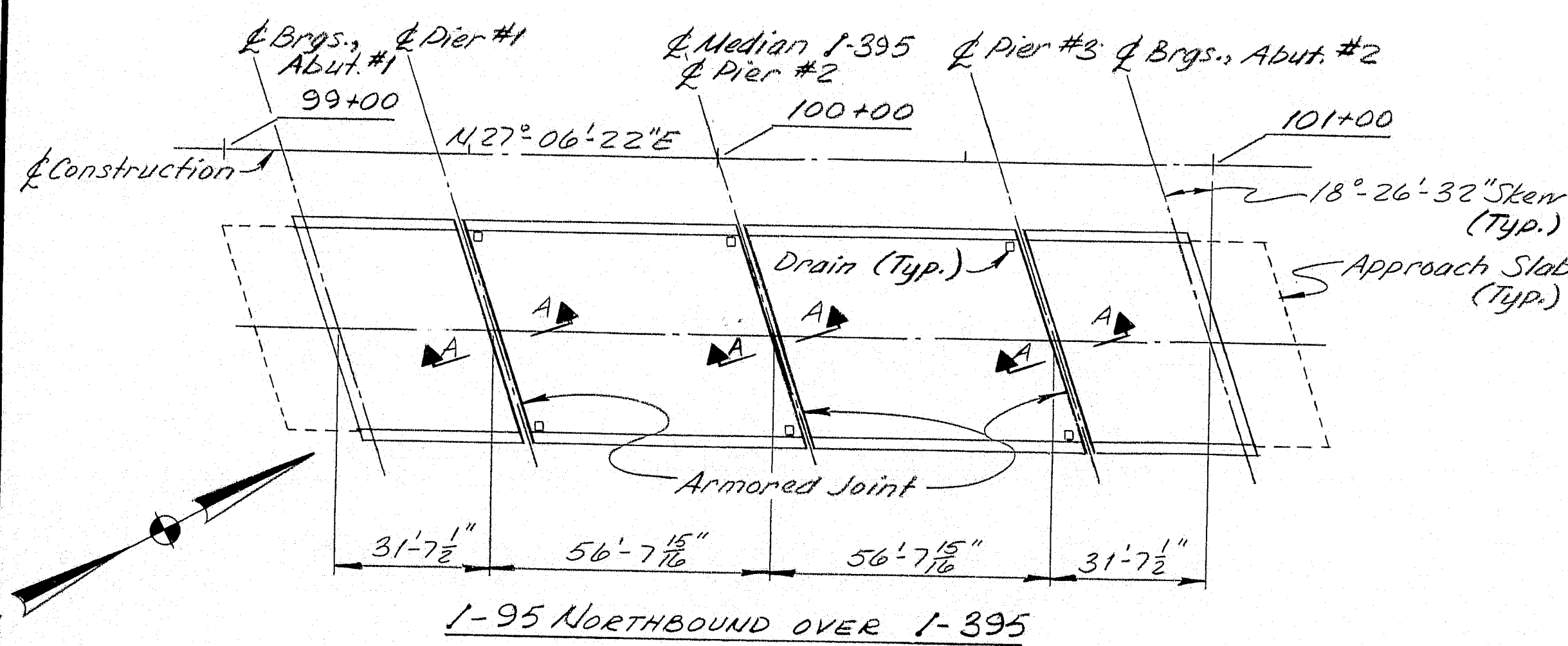
|                         |         |
|-------------------------|---------|
| PROJECT DESIGN ENGINEER | DATE    |
| DESIGN - DETAILED       | 12/2/81 |
| CHECKED                 | 1/2/82  |
| FIELD CHANGES           |         |



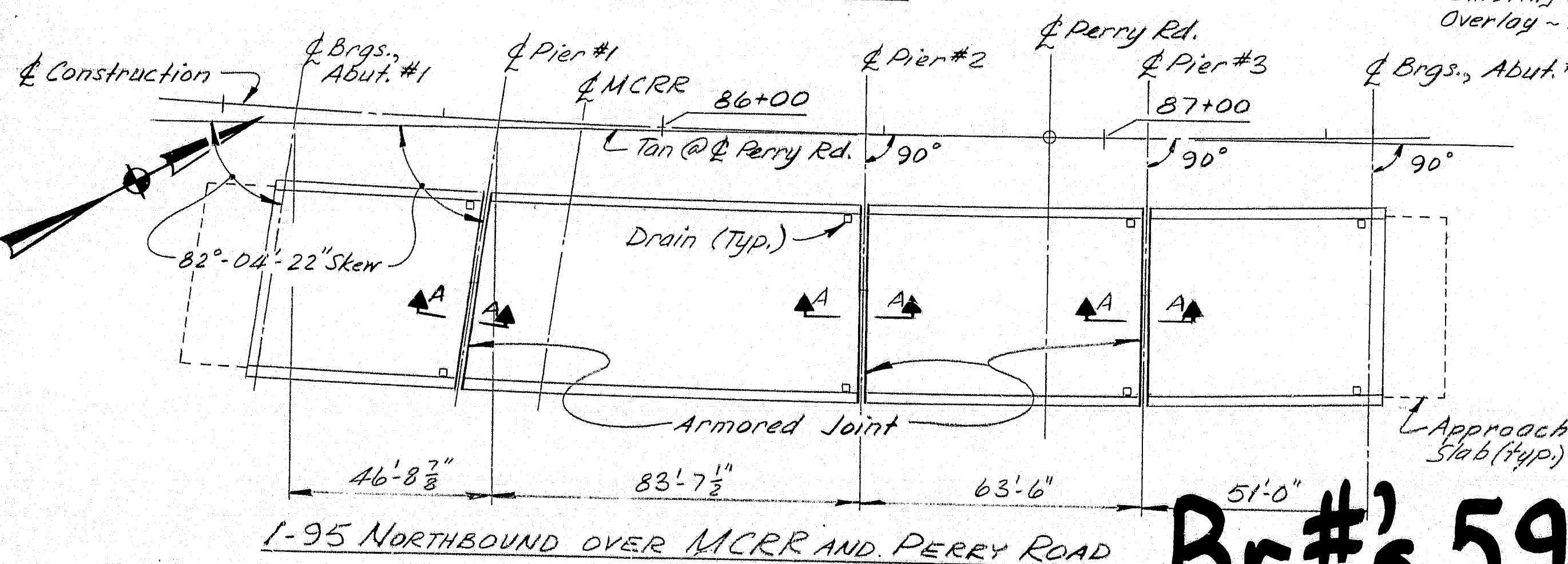
I-95 NORTHBOUND OVER STILLWATER AVE.



I-95 NORTHBOUND OVER BROADWAY



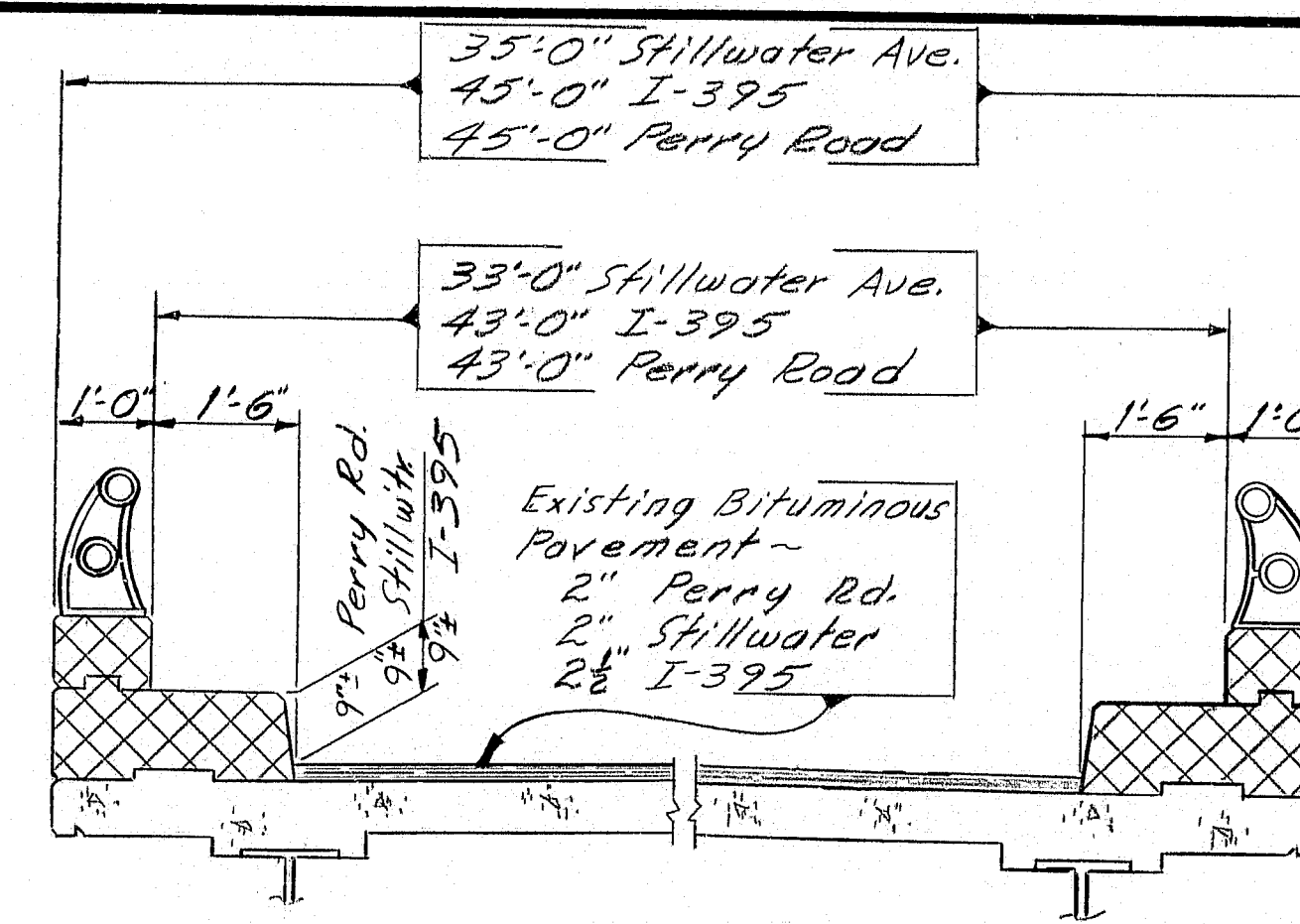
I-95 NORTHBOUND OVER I-395



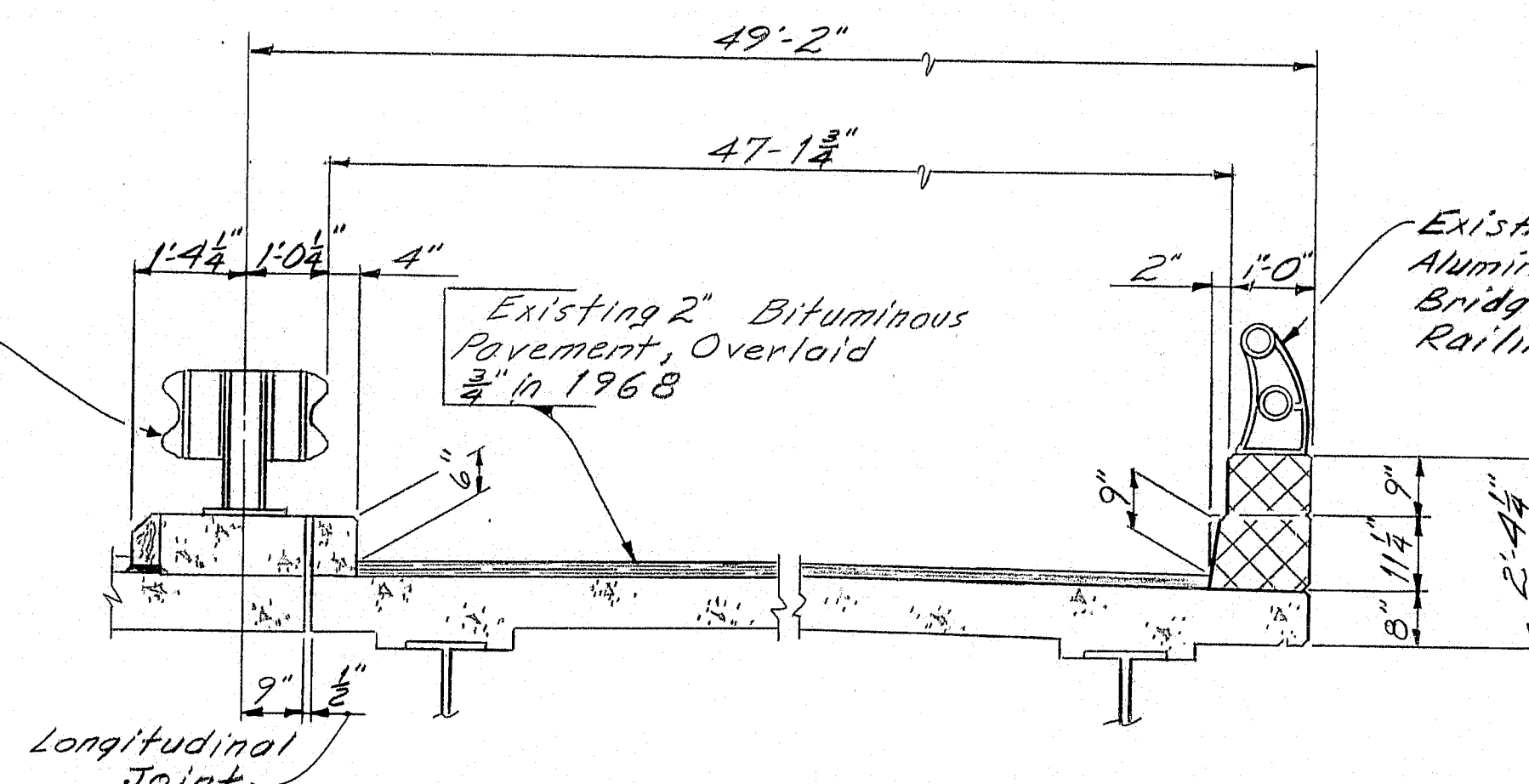
I-95 NORTHBOUND OVER MCR AND PERRY ROAD

Plans of the existing bridge are available for the Contractor's reference at the Bridge Design Office in Augusta. The plans are reproductions of original drawings as prepared for the construction of the bridge and it is very unlikely that the plans will show any construction field changes or any alterations which may have been made to the bridge during its life span.

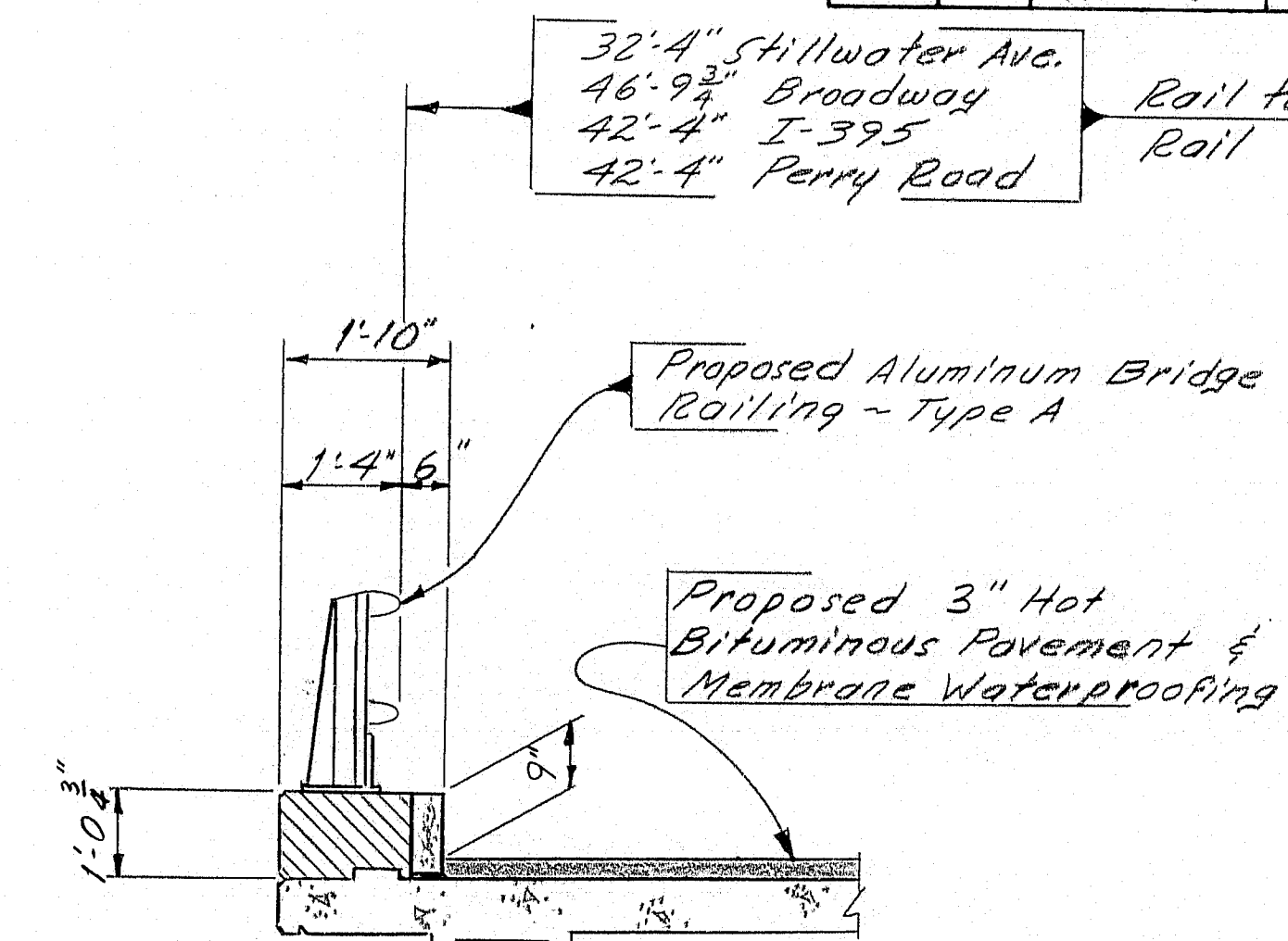
| SYMBOLS |  |  |  |
|---------|--|--|--|
|         |  |  |  |
|         |  |  |  |



TRANSVERSE SECTION



TRANSVERSE SECTION  
I-95 over Broadway



PROPOSED CURB SECTION

#### SPECIFICATIONS

DESIGN: Load Factor Design per AASHTO Standard Specifications for Highway Bridges 1977 and interims thru 1982.  
CONTRACT: State of Maine, Department of Transportation, Standard Specifications, Highways and Bridges, Revision of June 1981.

#### MATERIALS

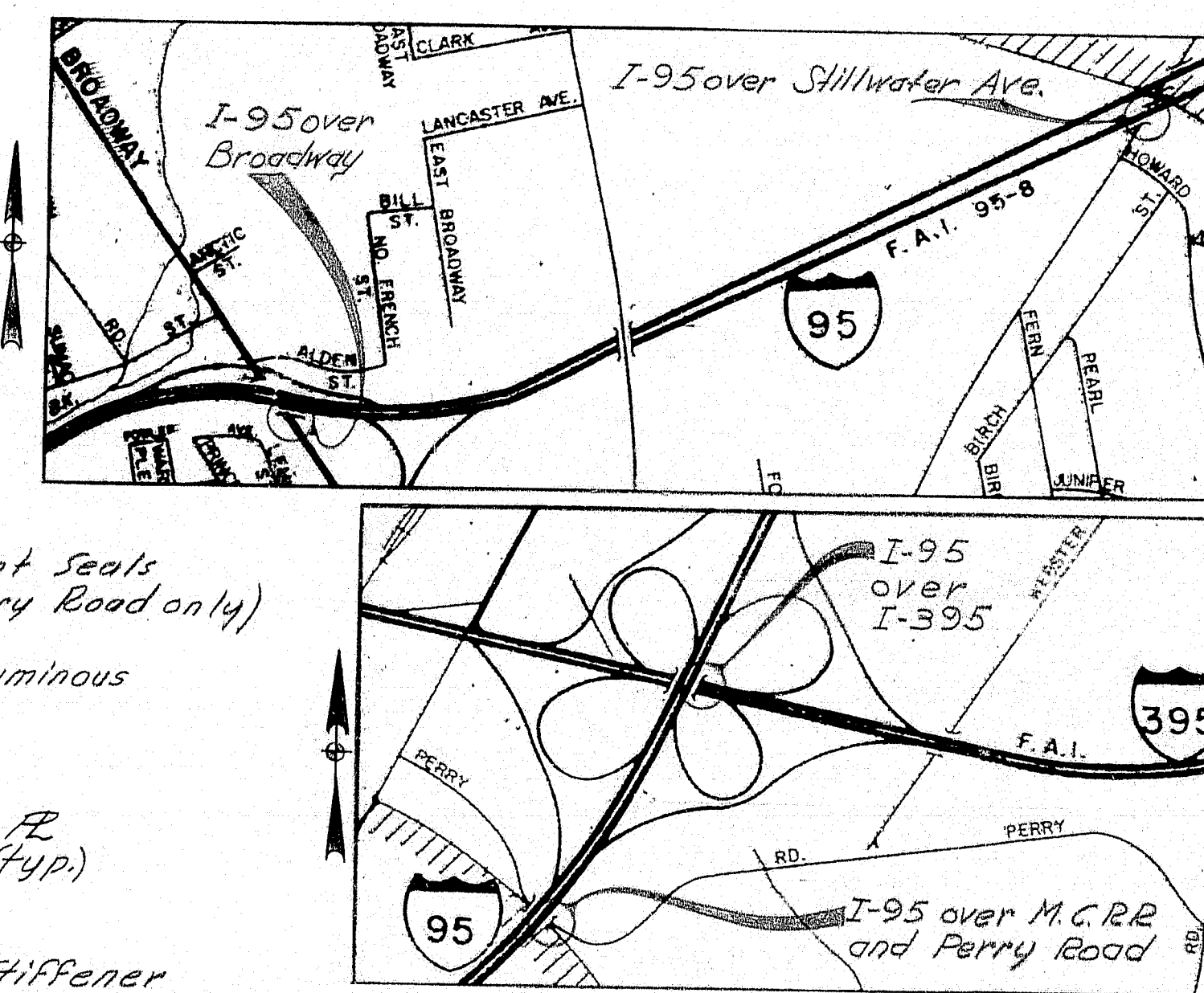
CONCRETE: Class "A"  
REINFORCING STEEL: ASTM A615, Grade 60

#### BASIC DESIGN STRESSES

CONCRETE:  $f'_c = 3000$  PSI  
REINFORCING STEEL:  $f_y = 60,000$  PSI

#### INDEX OF SHEETS

|  |       |
|--|-------|
| General Plan                                     | 1     |
| Estimated Quantities and Gen. Construction Notes | 2     |
| I-95 N.B. over MCR & Perry Road                  | 3     |
| I-95 N.B. over I-395                             | 4     |
| I-95 N.B. over Broadway                          | 5     |
| I-95 N.B. over Stillwater Avenue                 | 7 & 8 |
| Reinforcing Steel Schedule                       | 9     |
| Standard Details:                                |       |
| BD 114-81 Alum. Bridge Railing, 2" Bar           | 10    |
| BD 120-81 Conc. End Posts                        | 11    |
| BD 127-81 Temp. Conc. Barrier                    | 12    |



LOCATION MAPS  
Scale of feet

|  |
|--|
| STATE OF MAINE<br>DEPARTMENT OF TRANSPORTATION   |
| INTERSTATE 95 NORTHBOUND OVER<br>STILLWATER AVENUE<br>BROADWAY<br>INTERSTATE 395<br>MCR AND PERRY ROAD<br>BANGOR<br>GENERAL PLAN |
| SHEET 1 OF 12 AUGUSTA, MAINE   |

Br.#'s 5972, 5795, 5789, & 5800 R90-160

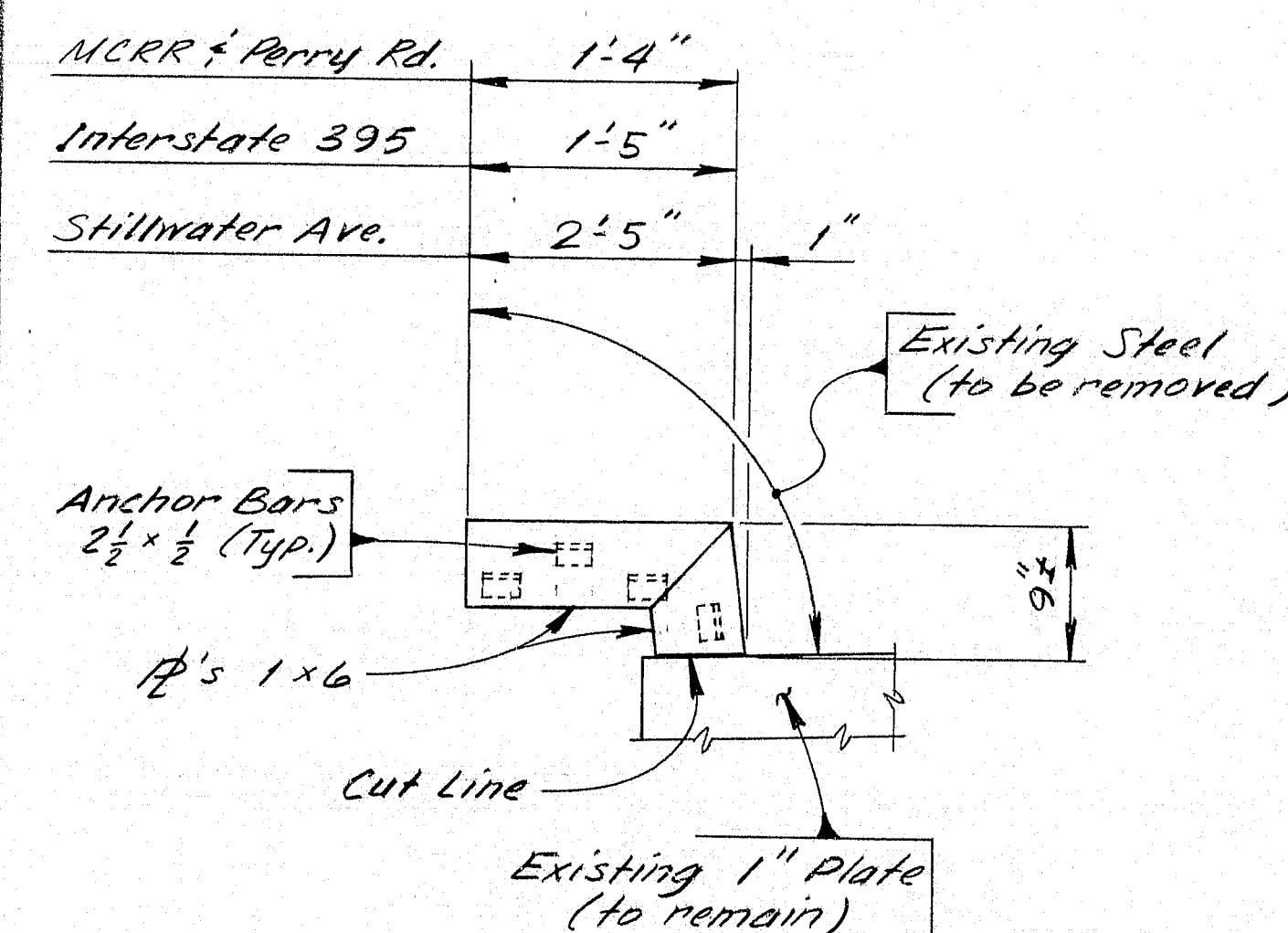


## ESTIMATED BRIDGE QUANTITIES

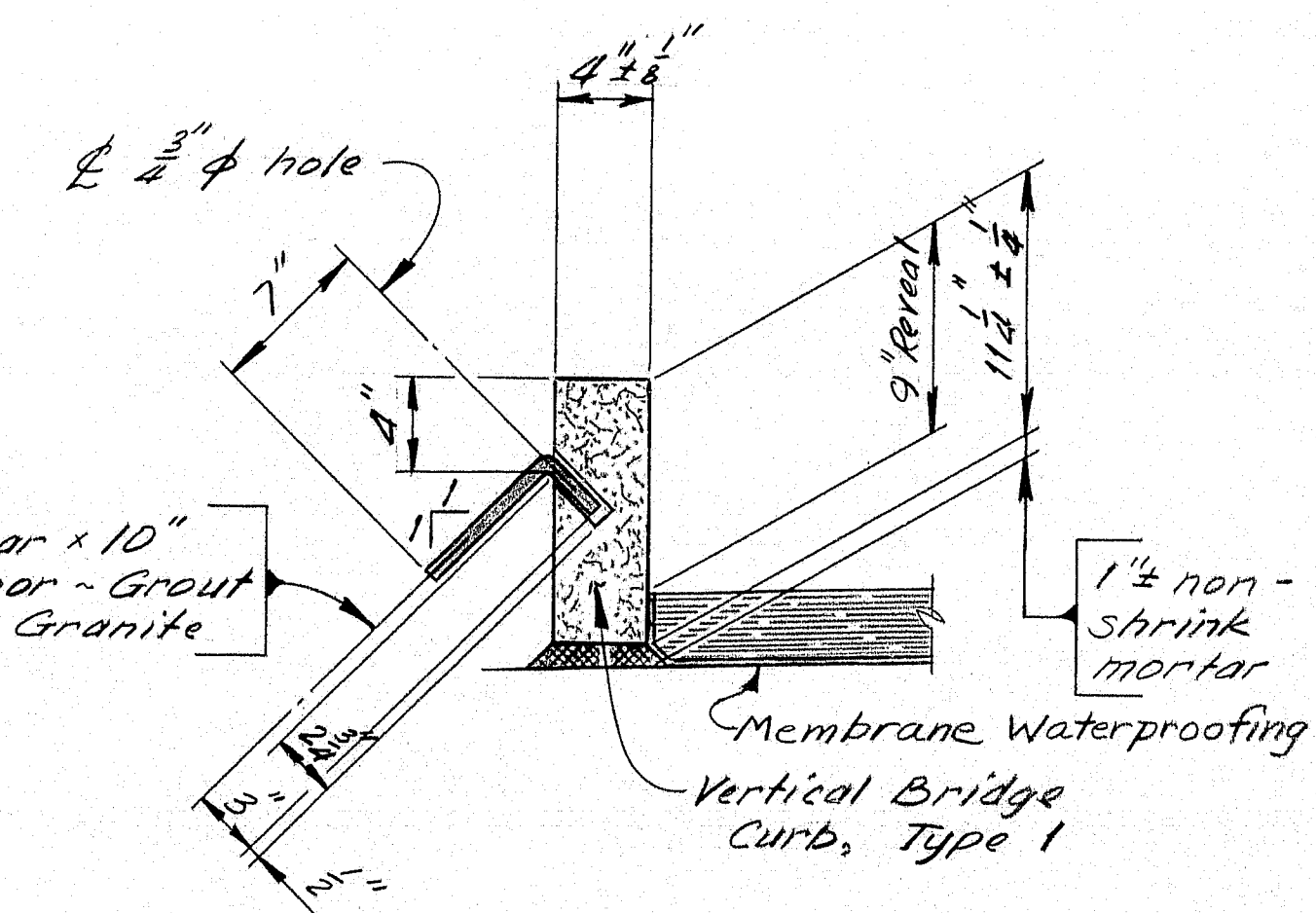
| Item No. | Description   | Unit | 1-95 N.B./MCR | 1-95 N.B./1-395 | 1-95 N.B./Broadway | 1-95 N.B./Stillwater | Total Quantity |
|----------|---|------|---------------|-----------------|--------------------|----------------------|----------------|
| 202.12   | Removal of Existing Structural Concrete                         | C.Y. | 75            | 43              | 10                 | 55                   | 183            |
| 202.13   | Removing Existing Railings (Retained by Department)             | L.F. | 504           | 358             | 155                | 433                  | 1450           |
| 202.201  | Removing Bituminous Pavement (including Membrane Waterproofing) | S.Y. | 1124          | 802             | 813                | 728                  | 3467           |
| 403.08   | Hot Bituminous Pavement, Grading "C"                            | Ton  | 192           | 137             | 132                | 126                  | 587            |
| 502.42   | Structural Concrete Roadway and Sidewalk Slabs on Steel Bridges | C.Y. | 32            | 23              | 10                 | 28                   | 93             |
| 503.12   | Reinforcing Steel Fabricated and Delivered                      | Lbs. | 3176          | 2313            | 940                | 3238                 | 9667           |
| 503.13   | Reinforcing Steel Placing                                       | Lbs. | 3176          | 2313            | 940                | 3238                 | 9667           |
| 507.092  | Aluminum Bridge Railing, 2-Bar                                  | L.F. | 485           | 340             | 146                | 415                  | 1386           |
| 508.10   | Membrane Waterproofing  | S.Y. | 1176          | 839             | 810                | 773                  | 3598           |
| 514.06   | Curing Box for Concrete Cylinders                               | Each | 0.25          | 0.25            | 0.25               | 0.25                 | 1              |
| 515.20   | Protective Coating for Concrete Surfaces                        | L.S. | 0.25          | 0.25            | 0.25               | 0.25                 | 1              |
| 520.2401 | Expansion Device Modification (MCR & Perry Rd.)                 | Each | 3             | —               | —                  | —                    | 3              |
| 520.2402 | Expansion Device Modification (1-395)                           | Each | —             | 3               | —                  | —                    | 3              |
| 520.2403 | Expansion Device Modification (Broadway Pier No. 1)             | Each | —             | —               | 1                  | —                    | 1              |
| 520.2404 | Expansion Device Modification (Broadway Pier No. 2)             | Each | —             | —               | 1                  | —                    | 1              |
| 520.2405 | Expansion Device Modification (Stillwater Ave. Pier No. 1)      | Each | —             | —               | —                  | 1                    | 1              |
| 520.2406 | Expansion Device Modification (Stillwater Ave. Pier No. 2)      | Each | —             | —               | —                  | 1                    | 1              |
| 526.30   | Temporary Concrete Barrier, Type 1                              | L.F. | 480           | 400             | 380                | 440                  | 1700           |
| 526.40   | Resetting Temporary Concrete Barrier, Type 1                    | L.F. | 480           | 400             | 380                | 440                  | 1700           |
| 606.364  | Guard Rail, Remove, Modify and Reset Type 3b                    | L.F. | 50            | —               | —                  | —                    | 50             |
| 606.367  | Replace Unusable Existing Guard Rail Posts                      | Each | 2             | —               | —                  | —                    | 2              |
| 609.13   | Vertical Bridge Curb, Type 1                                    | L.F. | 469           | 318             | 138                | 399                  | 1324           |
| 609.38   | Reset Curb Type 1   | L.F. | 80            | 80              | 40                 | 80                   | 280            |
| 629.05   | Hand Labor, Straight Time                                       | M.H. | 20            | 20              | 20                 | 20                   | 80             |
| 631.10   | Air Compressor (including operator)                             | Hr.  | 20            | 20              | 20                 | 20                   | 80             |
| 631.11   | Air Tool (including operator)                                   | Hr.  | 20            | 20              | 20                 | 20                   | 80             |
| 659.10   | Mobilization  | L.S. | 0.25          | 0.25            | 0.25               | 0.25                 | 1              |
| 660.21   | On-the-Job Training   | M.H. | —             | —               | —                  | —                    | —              |

## GENERAL CONSTRUCTION NOTES

- Chamfer all exposed edges of concrete a consistent dimension between  $\frac{1}{2}$ " and  $\frac{3}{4}$ " inclusive.
- Reinforcing steel shall have 2 inches cover unless otherwise indicated.
- Protective Coating for Concrete Surfaces shall be applied to all exposed surfaces of the new concrete curbs, fascias, concrete end posts, and concrete wearing surface at Stillwater Avenue and Broadway Pier No. 2 expansion joints.
- At all structures, the existing granite curbs on the approaches adjacent to the bridge curbs shall be reset for a length of about twenty feet each, as determined by the Engineer, to provide a transition to the new bridge curbs. Payment will be made under Item 609.38.
- Holes for grouting curb reinforcing shall be  $1\frac{1}{2}$ " to 2" in diameter, inclusive. Holes shall be filled with water for a minimum of two hours immediately before grouting, at which time all excess water shall be removed. The grouted area around the reinforcing bars shall be kept wet from the time of initial set for a minimum of twelve hours, with burlap or other suitable means. The grout shall be used in accordance with the manufacturer's recommendations and shall be included on the Department's list of Approved Non-shrink Grouts. No separate payment will be made for drilling and grouting, and all such work will be considered incidental to Item 503.13, Reinforcing Steel, Placing.
- The exact location and details of the construction joint for the Expansion Device Modifications, where required for stage construction, shall be approved by the Engineer.
- The existing sign supports on 1-95 N.B. over 1-395 shall be modified as shown on the plans. All work for modifying the sign supports will be considered incidental to Item 502.42.
- Field welding of expansion joints adjacent to in-place seals shall be done in such a manner to protect the seals from damage.
- Compression seals shall be plugged at all ends in a manner approved by the Engineer.



REMOVAL OF EXISTING CURB PLATES



GRANITE DETAIL

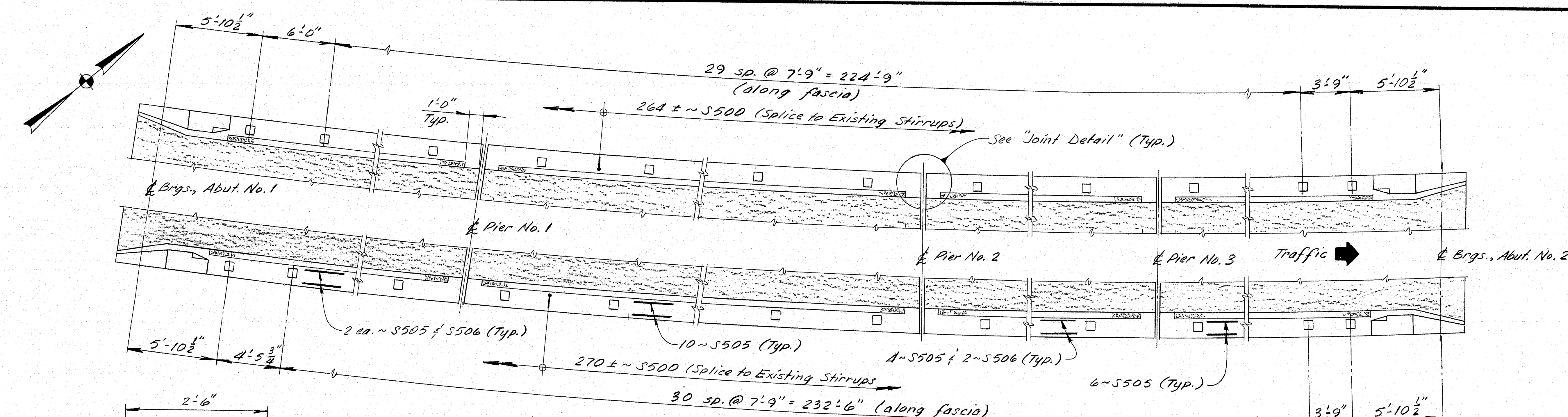
STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION  
INTERSTATE 95 NORTHBOUND  
OVER  
STILLWATER AVENUE  
BROADWAY  
INTERSTATE 395  
MCR AND PERRY ROAD  
BANGOR  
ESTIMATED QUANTITIES & CONST. NOTES  
SHEET 2 OF 12 AUGUSTA, MAINE  
IR-95-B(130) BANGOR

R90-161

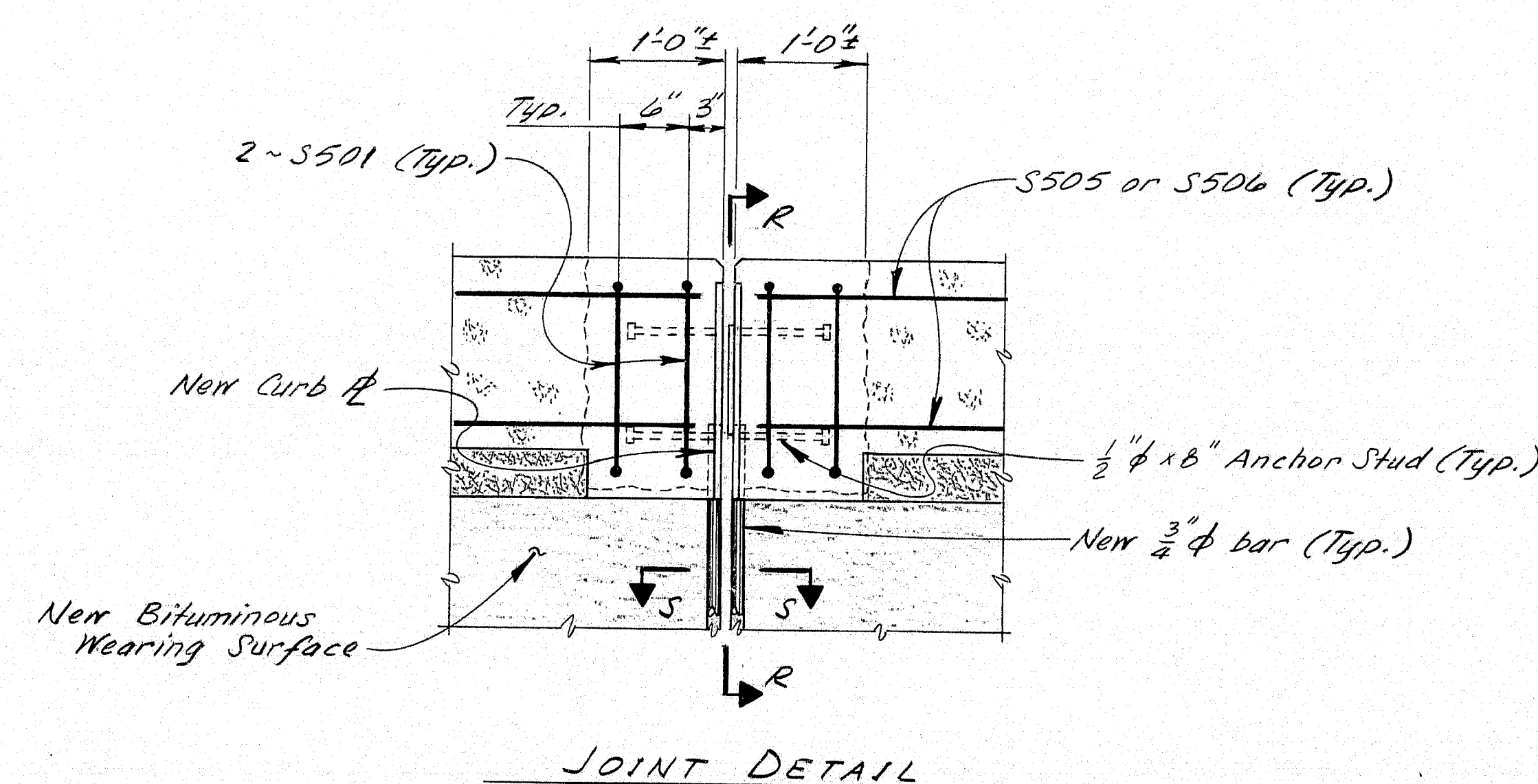
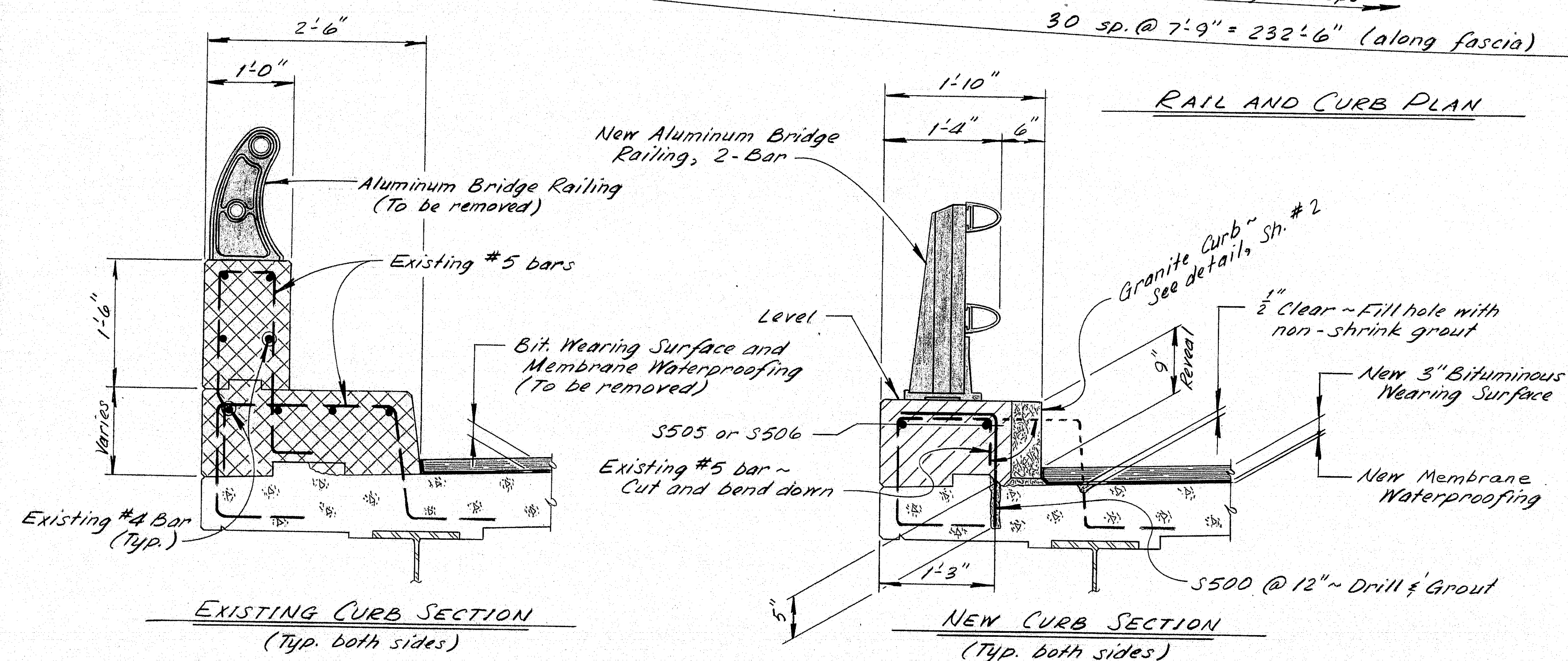


BRIDGE RAILING  
EXPANSION JOINTS

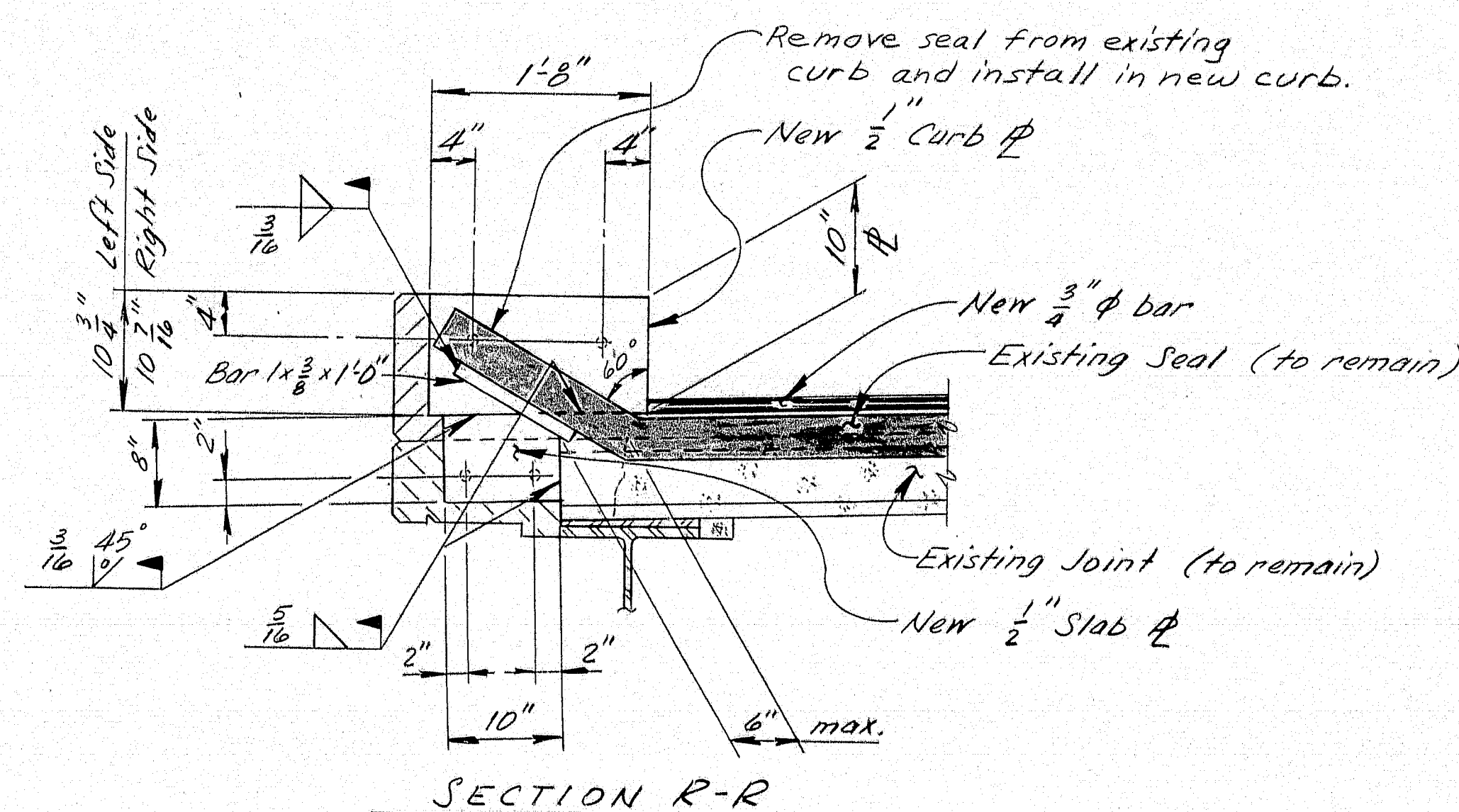
|                                      |
|--------------------------------------|
| Pier No. 1 = 1" @ 45                 |
| Pier No. 2 = $1\frac{5}{8}$ " @ 45   |
| Pier No. 3 = $1\frac{5}{8}$ " @ 45   |
| See BD 114-81 for<br>exact locations |



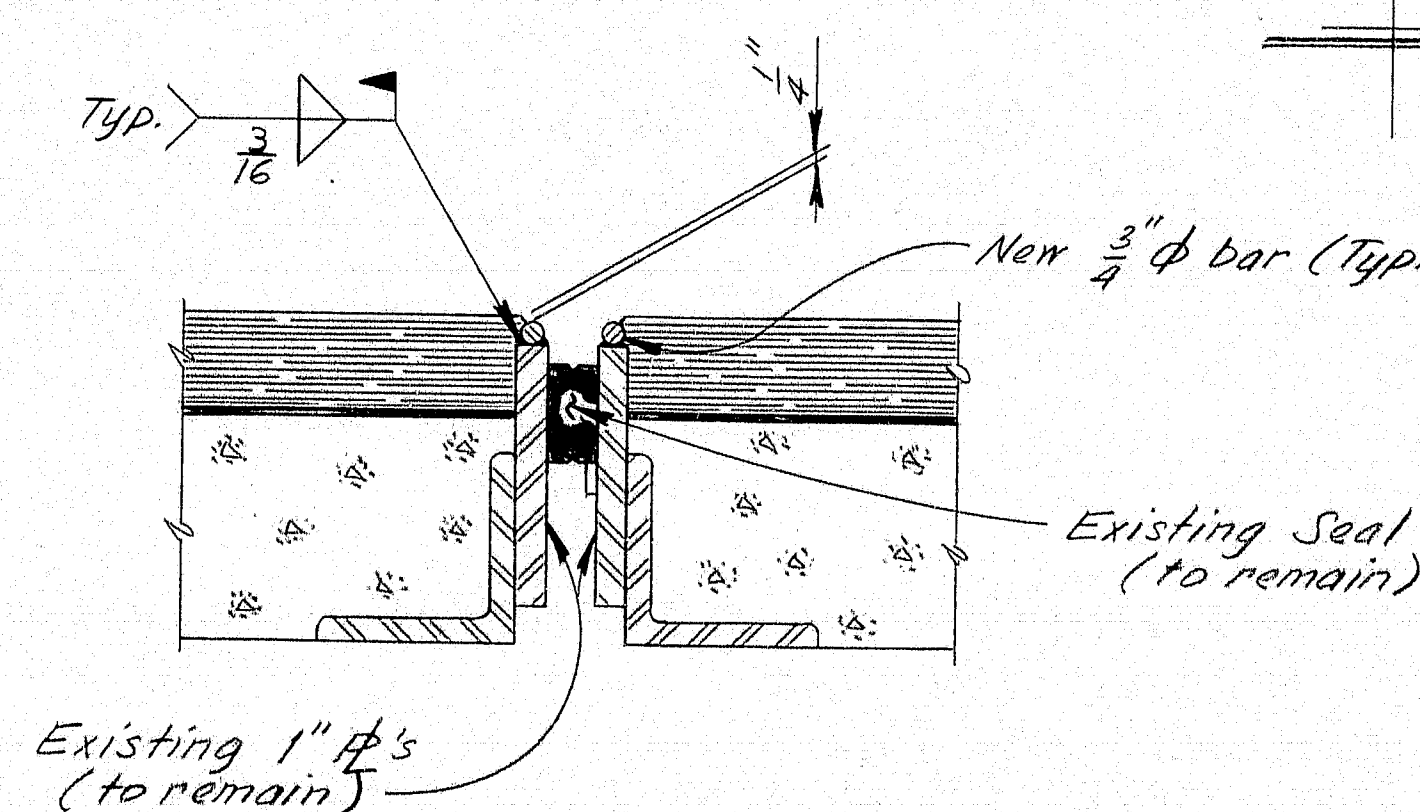
RAIL AND CURB PLAN



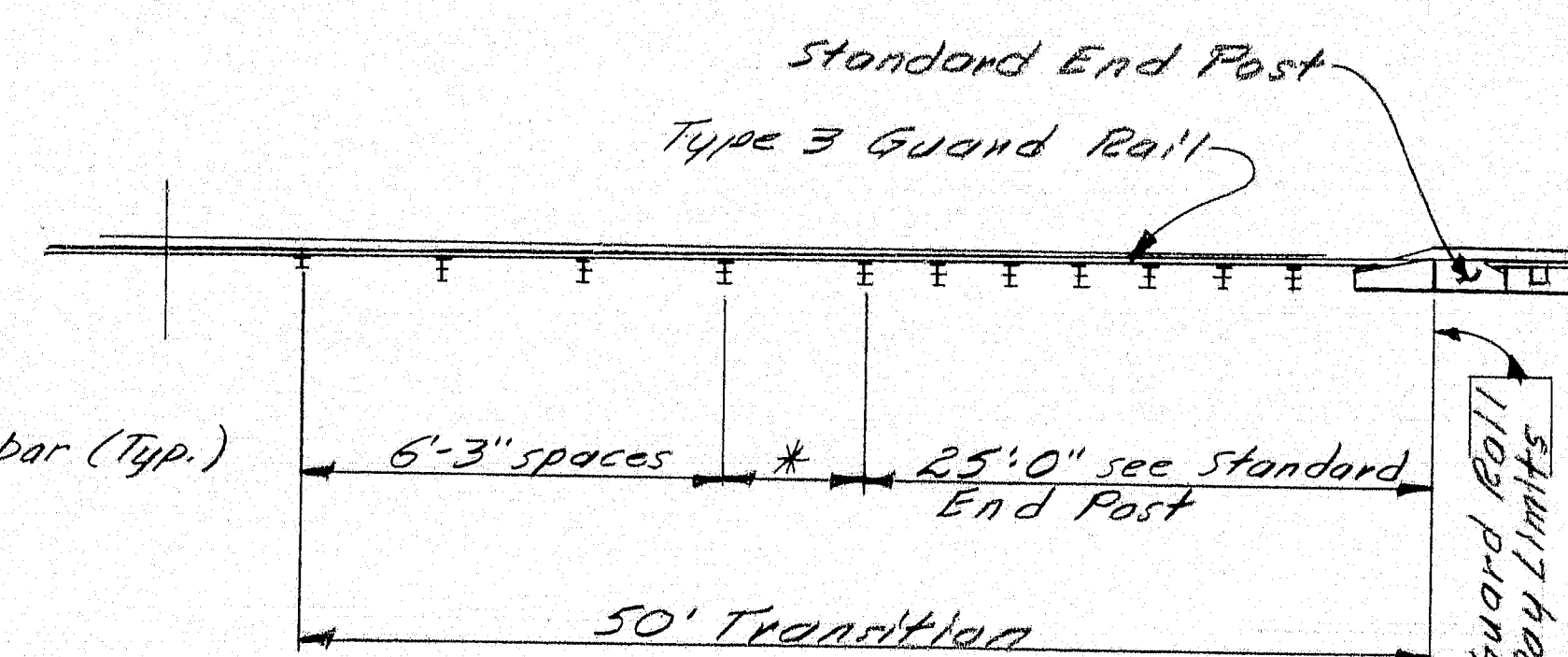
### JOINT DETAILS



SECTION R-



SECTION 5-5



GUARD RAIL TRANSITION  
(South approach railing only (typ. both sides))

\* = Modify Panel one or two spaces  
as required to match Rail.  
3'-1 1/2" min. to 6'-3" max. each

## REFERENCES

For Symbols, see Sheet No. 1  
For Removal of Existing Curb  
Plates, see Sheet No. 2

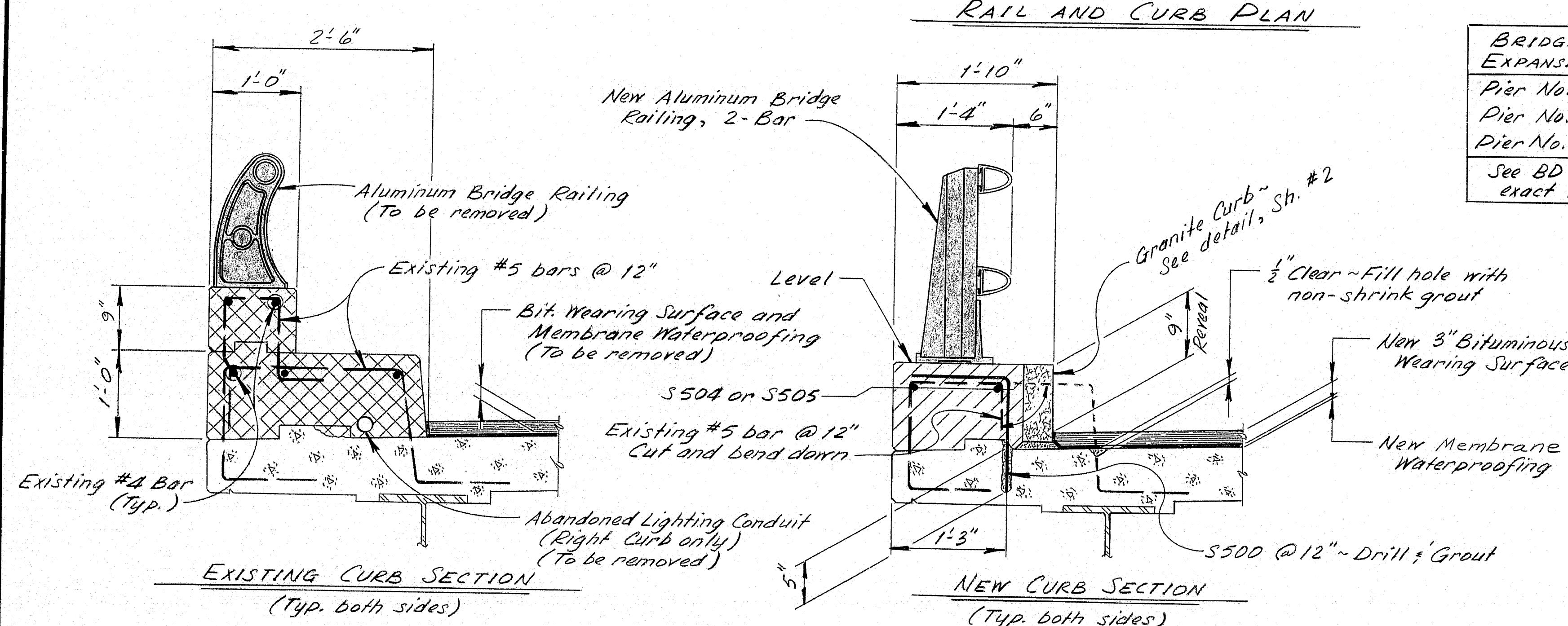
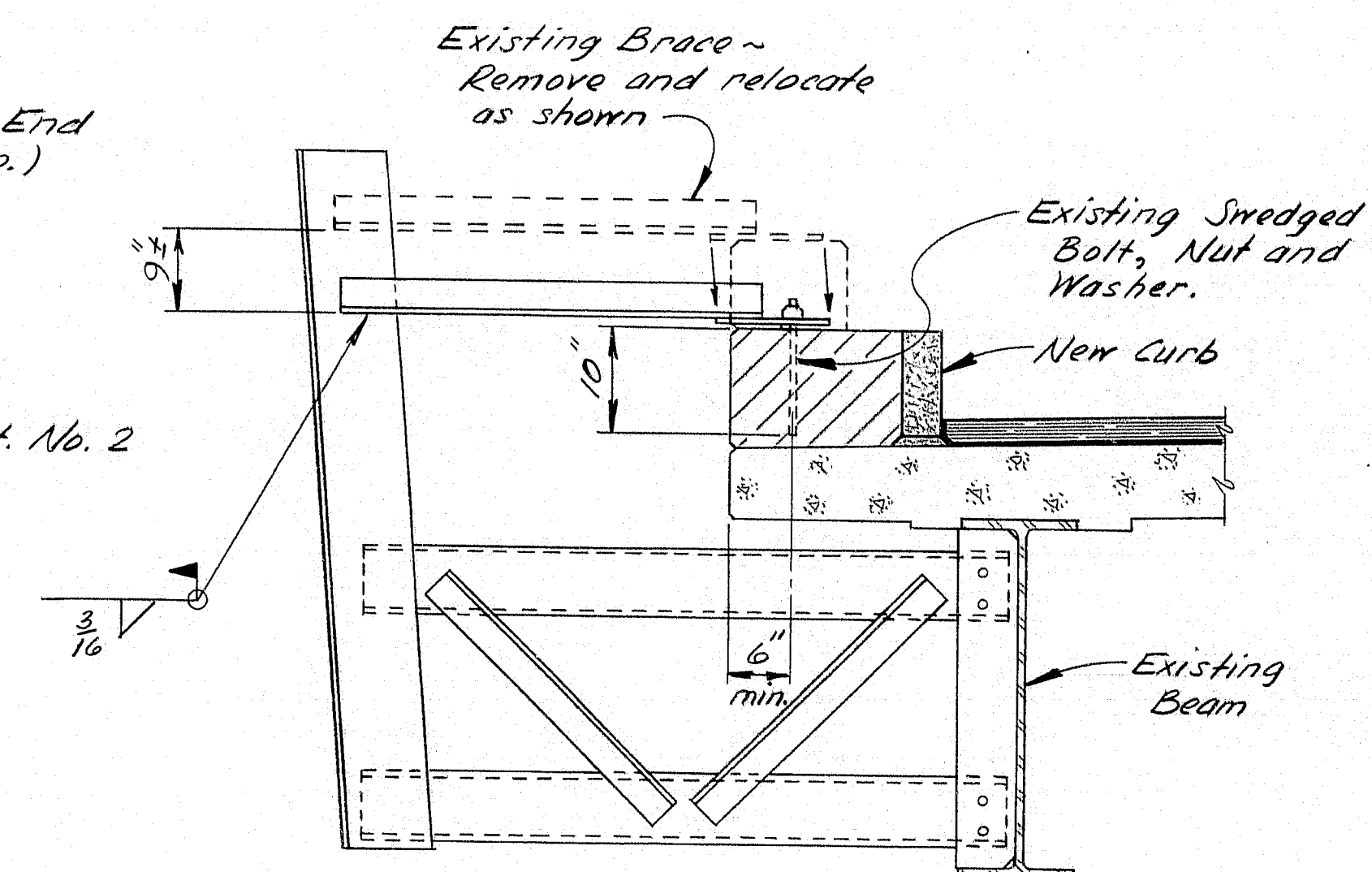
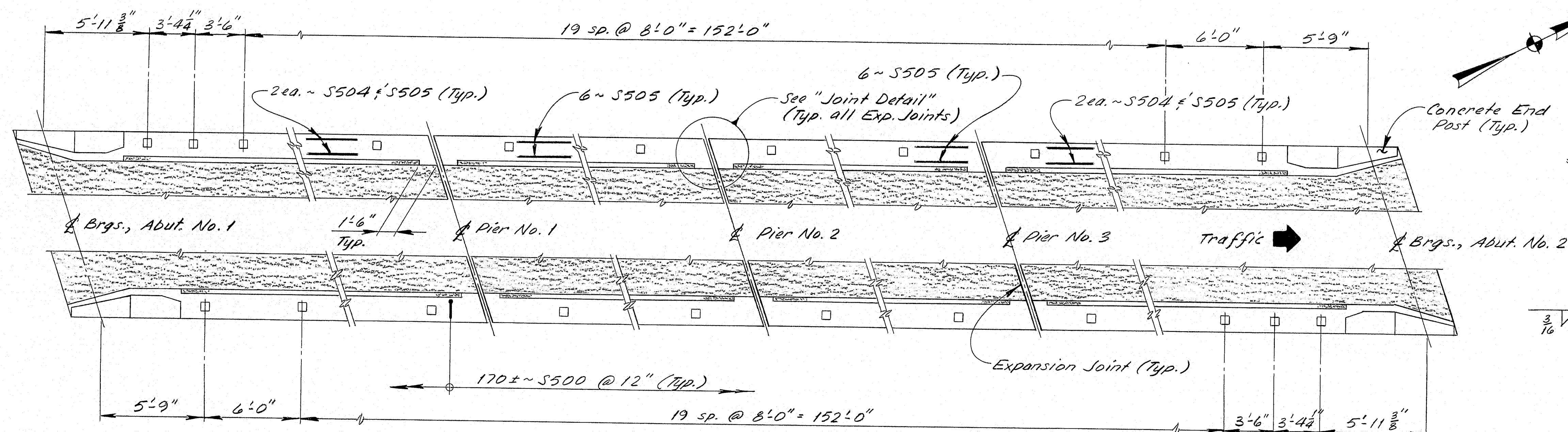
STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION

INTERSTATE 95  
NORTHBOUND  
OVER  
M.C.R.R. & PERRY ROAD  
BANGOR

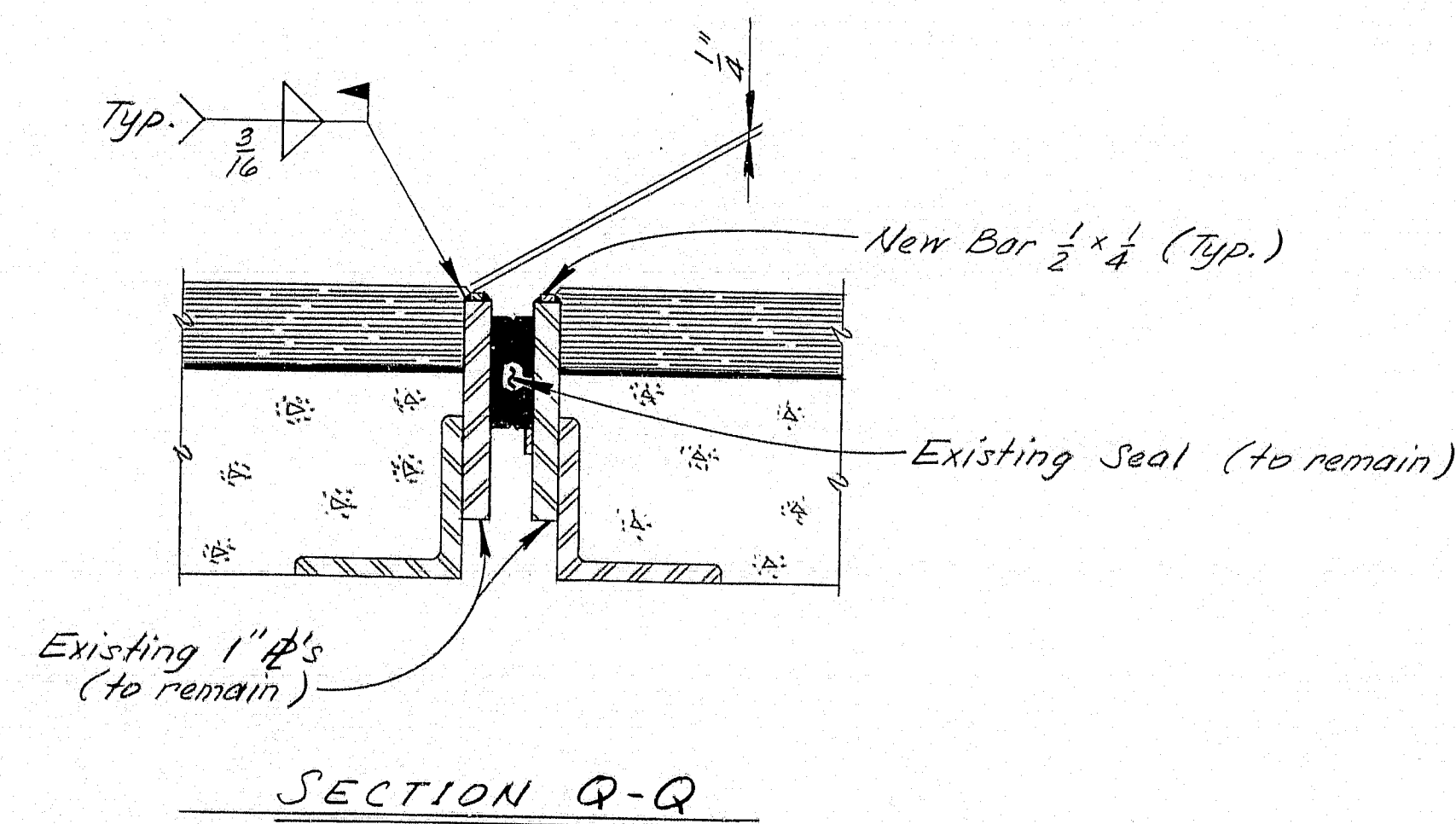
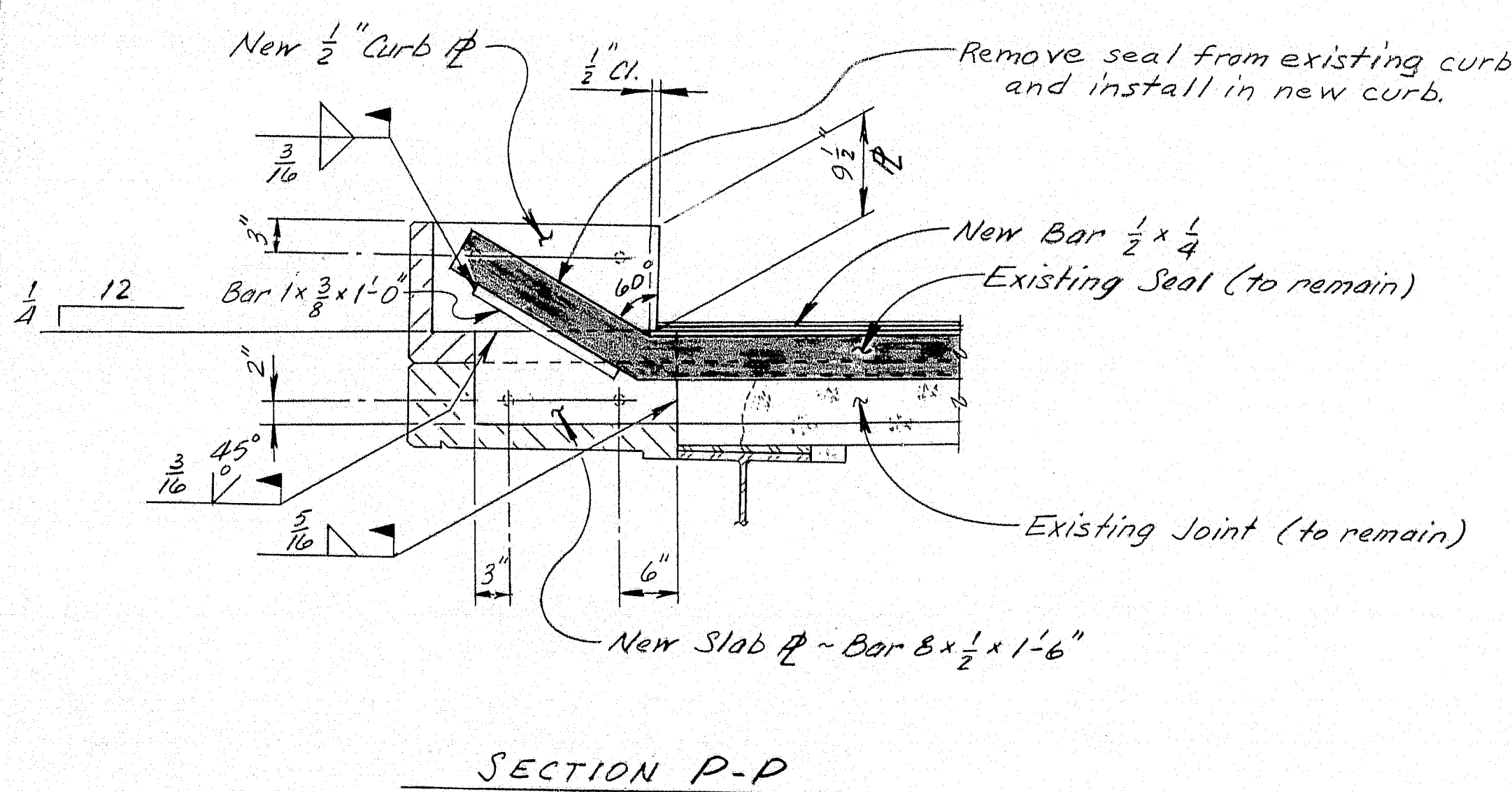
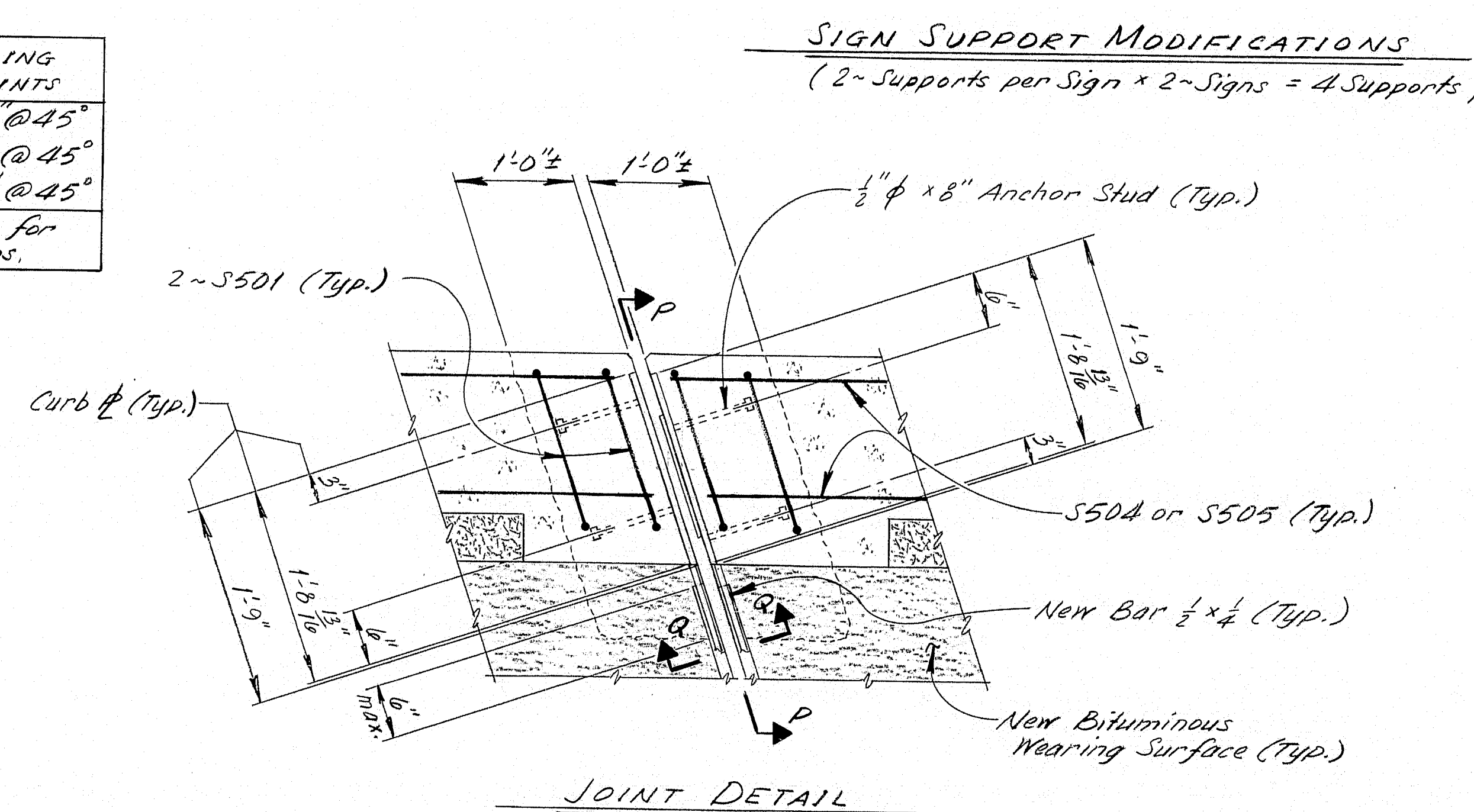
### BRIDGE DETAILS



| F.R.M.A.<br>REQ. NO. | STATE | PROJECT NUMBER | SHEET NO. | TOTAL SHEETS |
|----------------------|-------|----------------|-----------|--------------|
| 1                    | MAINE | 2R-85-8(30)175 | 21        | 34           |



| BRIDGE RAILING EXPANSION JOINTS    |              |
|------------------------------------|--------------|
| Pier No. 1 =                       | 1 1/4" @ 45° |
| Pier No. 2 =                       | 3/4" @ 45°   |
| Pier No. 3 =                       | 1 1/4" @ 45° |
| See BD 114-81 for exact locations. |              |



REFERENCES

For Symbols, see Sheet No. 1

For Removal of Existing Curb Plates, see Sheet No. 2

STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION

INTERSTATE 95  
NORTHBOUND  
OVER  
INTERSTATE 395  
BANGOR

BRIDGE DETAILS

| PROJECT DESIGN ENGINEER | DATE          |
|-------------------------|---------------|
| BY [Signature]          | 10/23/85      |
| DESIGN - CHECKED        | REVISIONS     |
| 13-000                  | 5/2/83        |
| PLANS                   | FIELD CHANGES |

BRUNING 44.132.45710



BRIDGE RAILING  
EXPANSION JOINTS  
Pier No. 1 = 1' @ 45°  
Pier No. 2 = 1½' @ 45°  
See BD 114-81 for  
exact locations.

Hand-drawn plan view of a bridge structure, labeled "RAIL AND CURB PLAN". The drawing shows a cross-section of the bridge with various components and dimensions.

**Labels and Dimensions:**

- Top Section:**
  - Median, I-95
  - Deck of Span 1's replaced in 1983 - JHE
  - Existing Median
  - Traffic (indicated by an arrow pointing right)
- Bridge Structure:**
  - Brgs., Abut. No. 1
  - Pier No. 1
  - Pier No. 2
  - Brgs., Abut. No. 2
  - Concrete End Post (Typ.)
- Reinforcement and Details:**
  - 4~S505
  - 10~S505
  - See "Joint Detail"
  - 1'-6" TYP.
- Dimensions:**
  - 5'-5 $\frac{3}{4}$ "
  - 5'-8 $\frac{1}{2}$ "
  - 3 sp. @ 7'-0" = 21'-0"
  - 11 sp. @ 8'-0" = 88'-0"
  - 3 sp. @ 7'-0" = 21'-0"
  - 5'-6"
  - 5'-10"

**EXISTING CURB SECTION**  
(N.B. side only)

1'-2"  
1'-0" 2"  
Aluminum Bridge Railing (To be removed)  
Existing #5 bars (Typ.)  
Bit. Wearing Surface and Membrane Waterproofing (To be removed)  
1'-0" 9" 11 1/4"

**NEW CURB SECTION**  
(N.B. side only)

1'-10"  
1'-4" 6"  
New Aluminum Bridge Railing, 2-Bar  
Level  
S505  
Existing #5 Bar @ 6" ~ Cut and bend over both sides as shown  
Granite Curb ~ #2 See detail, Sh. #2  
9" Kerf  
New 3" Bituminous Wearing Surface  
New Membrane Waterproofing

Remove  $\frac{3}{4}$ "  $\phi$  bars in this area

Anchor Stud

$\frac{1}{8}$ "  $\phi$  Curb  $\phi$

$\frac{1}{8}$ " 12

Plumb Cut (Typ.)

60°

10"  $\phi$

Finished Grade

Compression Seal

Top of Slab

Existing  $\phi$  1 x 8 (to remain)

Typ. at all retention bars

$\frac{3}{16}$ "

$\frac{1}{8}$ "

SECTION J-J

## REFERENCES

For Symbols, see Sheet No. 1  
For Removal of Existing Curb  
Plates, see Sheet No. 2

STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION

INTERSTATE 95  
NORTHBOUND  
OVER  
BROADWAY  
BANGOR

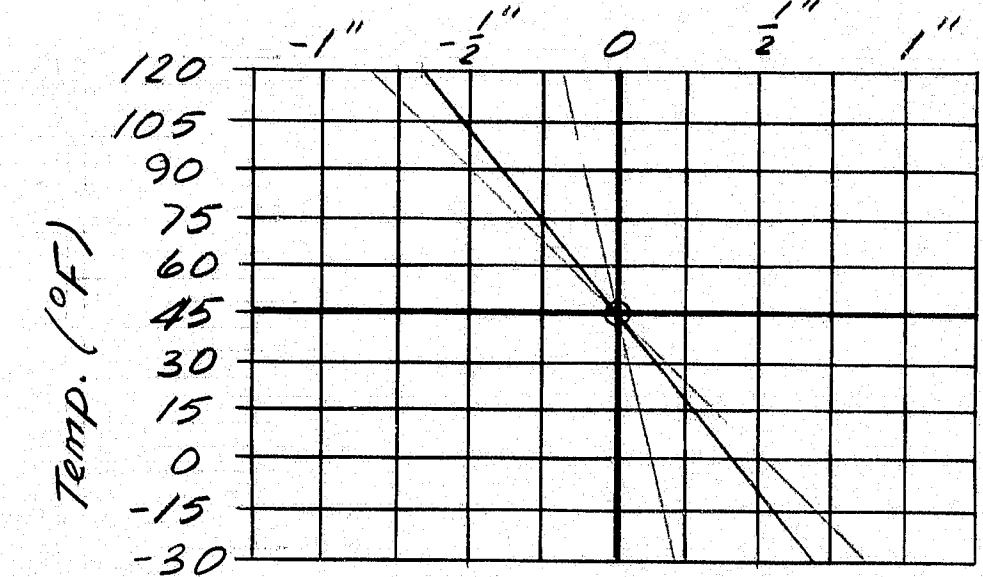
BRIDGE DETAILS  
PIER No. 1 EXPANSION JOINT

SHEET 5 OF 12 AUGUSTA, MAINE  
(130) BANGOR

**R90-164**



| STATE | PROJECT NUMBER    | SHEET NO. | TOTAL SHEETS |
|-------|-------------------|-----------|--------------|
| MAINE | 118-95-B (30) 175 | 23        | 34           |



COMPRESSION SEAL ADJUSTMENT CHART

- The seals to be furnished shall have a minimum Movement Rating of:  
Pier No. 2 =  $1\frac{1}{2}$ "
- The seal shall be approved by the Engineer prior to installation.
- The joint opening will vary depending on the dimensions of the seal selected by the Contractor. The joint opening shall be set according to the opening shown on the approved shop detail drawings.
- The Compression Seal Adjustment Chart shows the adjustment necessary to set the joint opening shown on the shop detail drawings for temperatures other than 45°F. Adjustment is to be measured parallel to the centerline of construction.

REFERENCES

For Symbols, see Sheet No. 1  
For Removal of Existing Curb Plates, see Sheet No. 2

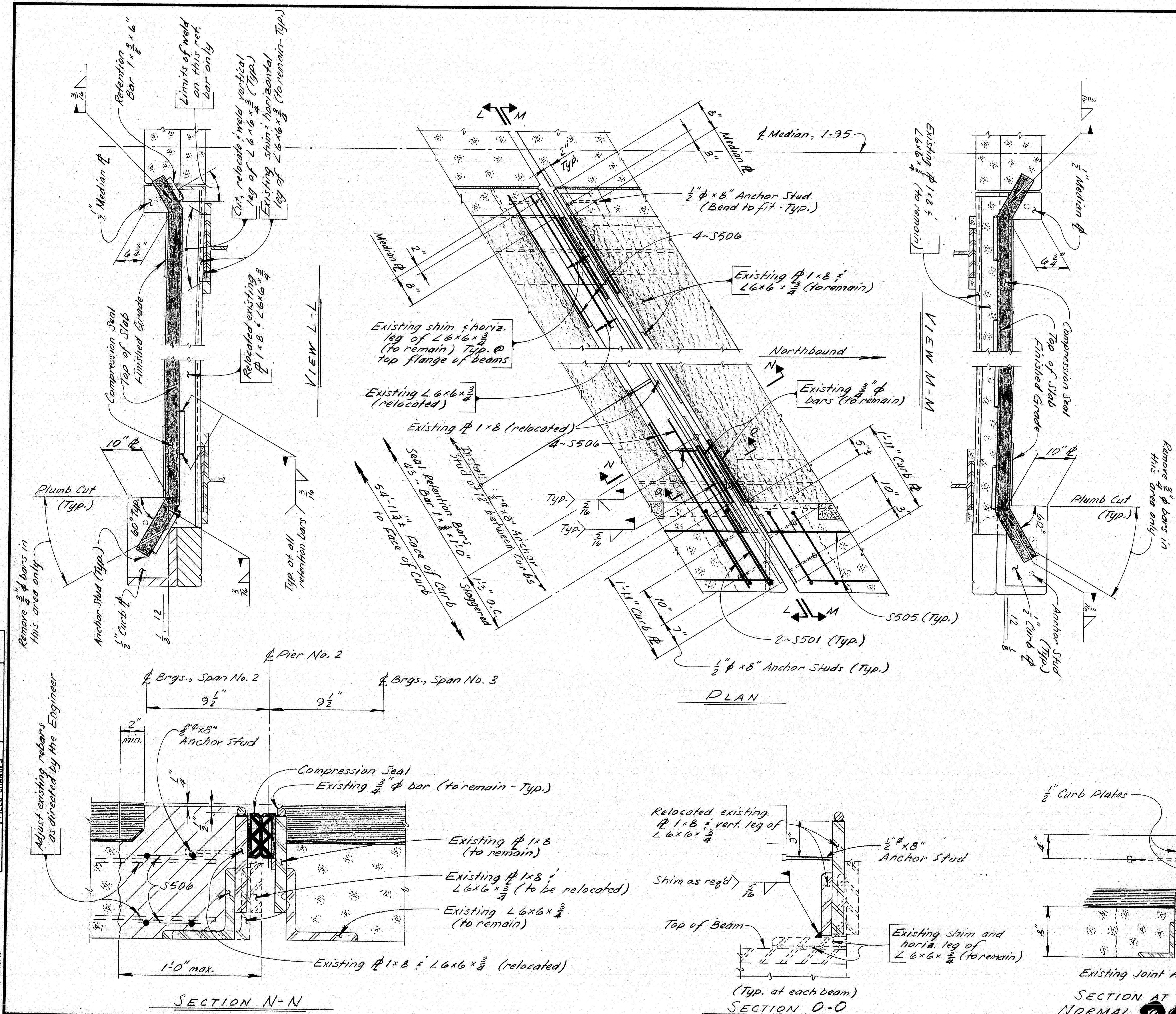
STATE OF MAINE  
DEPARTMENT OF TRANSPORTATION

INTERSTATE 95  
NORTHBOUND  
OVER  
BROADWAY  
BANGOR

PIER No. 2 EXPANSION JOINT

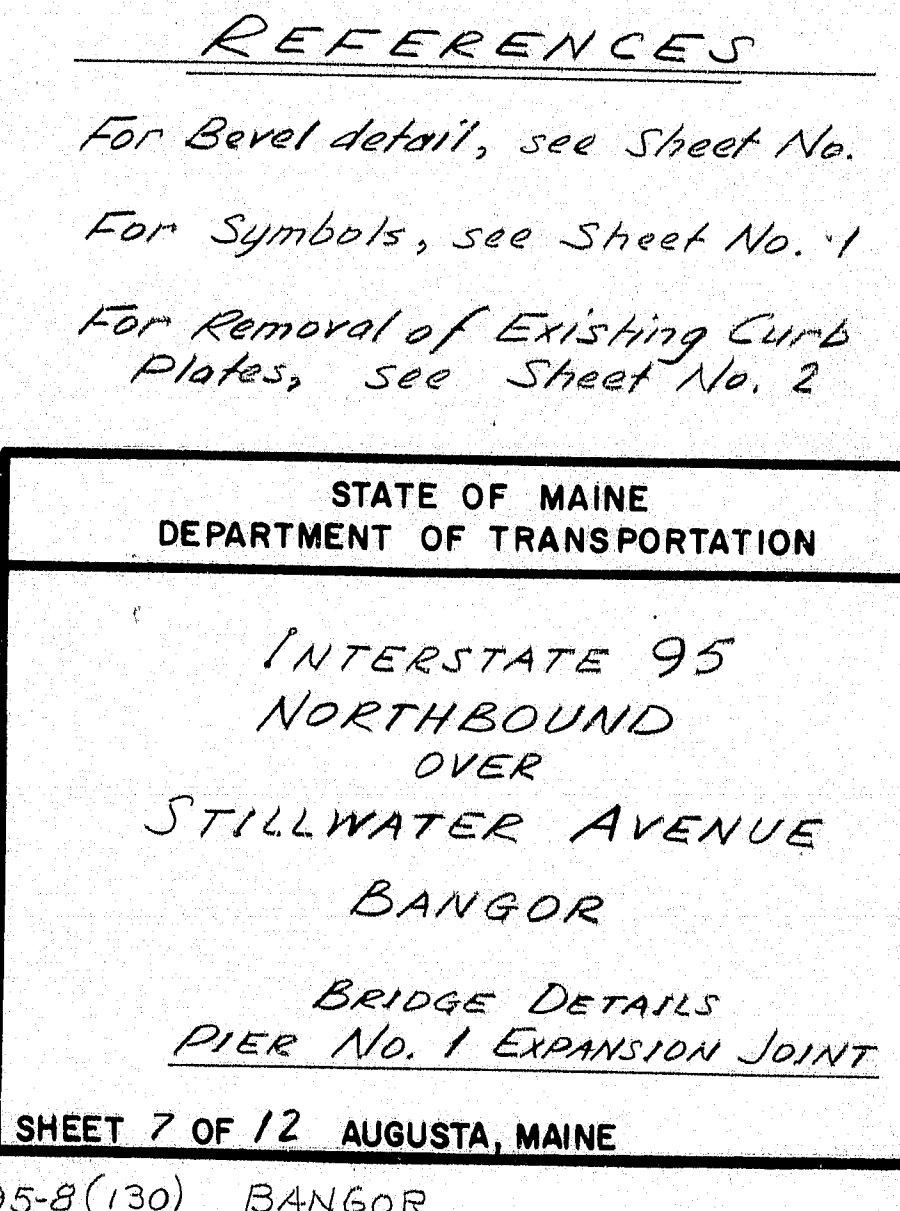
SHEET 6 OF 12 AUGUSTA, MAINE

118-95-B (130) BANGOR





|   |
|---|
| BRIDGE RAILING<br>EXPANSION JOINTS      |
| Pier No. 1 = $1\frac{1}{4}" @ 45^\circ$ |
| Pier No. 2 = $1\frac{5}{8}" @ 45^\circ$ |
| See BD 114-81 for<br>exact locations    |

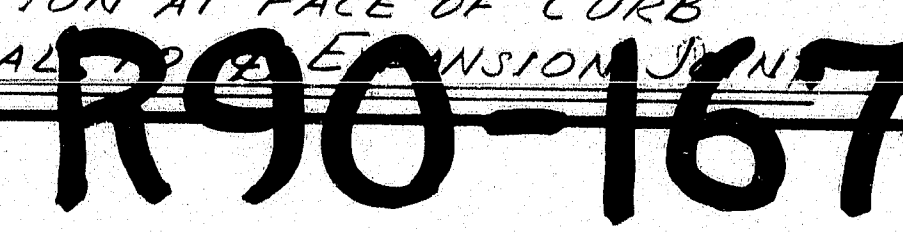


~~R90-166~~



|   |                         |                  |                |
|---|-------------------------|------------------|----------------|
| <b>PLANS</b>  | PROJECT DESIGN ENGINEER | BY               | DATE           |
|   | DESIGN - DETAILED       |                  |                |
|   | CHECKED                 | <i>D. Danner</i> | <i>Apr. 83</i> |
|   | REVISIONS               | <i>Revised</i>   | <i>5/83</i>    |
| PREP. BY: _____<br>CHECKED BY: _____<br>DATE: _____ |                         |                  |                |

TRAINING 44-132 15710



IR-95-R(130) BANGOR

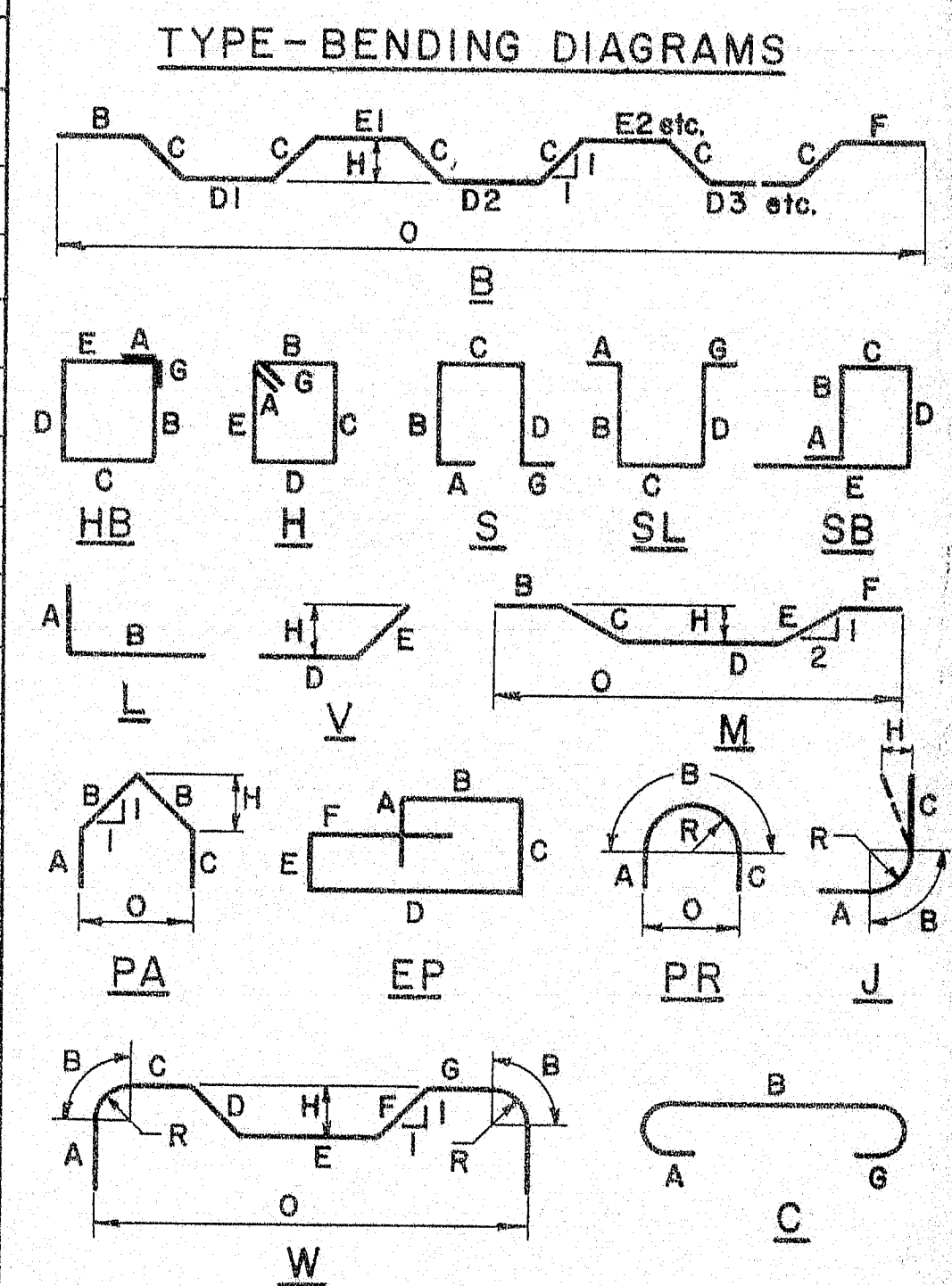


# REINFORCING STEEL SCHEDULE

| STRAIGHT BARS     |     |        |                     | BENT BARS  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|-------------------|-----|--------|---------------------|--|-----|--------|----------|------|-----|--------|----------|-------------------|-----|--------|------|-------|--------|--------|--------|-------|---|----|---|---|---|----------|------------|------------|--|--|--|
| MARK              | NO. | LENGTH | LOCATION            | MARK   | NO. | LENGTH | LOCATION | MARK | NO. | LENGTH | LOCATION | MARK              | NO. | LENGTH | TYPE | A     | B      | C      | D      | E     | F | G  | H | O | R | LOCATION |            |            |  |  |  |
| MCRR & PERRY ROAD |     |        |                     |  |     |        |          |      |     |        |          | MCRR & PERRY ROAD |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
| S505              | 44  | 20'-0" | Curbs               |  |     |        |          |      |     |        |          | S500              | 534 | 2'-5"  | L    | 1'-3" | 1'-2"  | -      | -      | -     | - | -  | - | - | - | -        | Curbs      |            |  |  |  |
| S506              | 8   | 30'-0" | Curbs               |  |     |        |          |      |     |        |          | S501              | 24  | 4'-0"  | S    | 0     | 1'-3"  | 1'-6"  | 1'-3"  | -     | - | 0  | - | - | - | -        | Curbs      |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
| INTERSTATE 395    |     |        |                     |  |     |        |          |      |     |        |          | INTERSTATE 395    |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
| S504              | 8   | 15'-0" | Curbs               |  |     |        |          |      |     |        |          | S500              | 340 | 2'-5"  | L    | 1'-3" | 1'-2"  | -      | -      | -     | - | -  | - | - | - | -        | Curbs      |            |  |  |  |
| S505              | 32  | 20'-0" | Curbs               |  |     |        |          |      |     |        |          | S501              | 24  | 4'-1"  | S    | 0     | 1'-3"  | 1'-2"  | 1'-3"  | -     | - | 0  | - | - | - | -        | Curbs      |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
| BROADWAY          |     |        |                     |  |     |        |          |      |     |        |          | BROADWAY          |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
| S505              | 18  | 20'-0" | Curbs               |  |     |        |          |      |     |        |          | S501              | 8   | 4'-3"  | S    | 0     | 1'-3"  | 1'-9"  | 1'-3"  | -     | - | 0  | - | - | - | -        | Curbs      |            |  |  |  |
| S506              | 8   | 30'-0" | Expansion Joint     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
| STILLWATER AVE.   |     |        |                     |  |     |        |          |      |     |        |          | STILLWATER AVE.   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
| S505              | 8   | 20'-0" | Curbs               |  |     |        |          |      |     |        |          | S500              | 520 | 2'-5"  | L    | 1'-3" | 1'-2"  | -      | -      | -     | - | -  | - | - | - | -        | Curbs      |            |  |  |  |
| S506              | 4   | 30'-0" | Exp. Joints         |  |     |        |          |      |     |        |          | S501              | 16  | 4'-4"  | S    | 0     | 1'-3"  | 1'-10" | 1'-3"  | -     | - | 0  | - | - | - | -        | Curbs      |            |  |  |  |
| S507              | 24  | 40'-0" | Curbs & Exp. Joints |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
| END POSTS         |     |        |                     |  |     |        |          |      |     |        |          | END POSTS         |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
| EP401             | 112 | 1'-10" | Dowels              | <div>Note: Quantities are given for a total of 14 End Posts - 4 each for I-95 over MCRR, I-395 and Stillwater Ave., and 2 for I-95 over Broadway</div> |     |        |          |      |     |        |          | EP402             | 56  | 4'-9"  | S    | 0     | 2'-1"  | 7"     | 2'-1"  | -     | - | 0  | - | - | - | -        | Horizontal |            |  |  |  |
| EP405             | 56  | 1'-5"  | Vertical            |  |     |        |          |      |     |        |          | EP403             | 56  | 4'-8"  | H    | 4"    | 1'-0"  | 1'-0"  | 1'-0"  | 1'-0" | - | 4" | - | - | - | -        | -          | Horizontal |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          | EP404             | 56  | 3'-1"  | S    | 0     | 1'-3"  | 7"     | 1'-3"  | -     | - | 0  | - | - | - | -        | -          | Vertical   |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          | EP408             | 42  | 4'-3"  | S    | 0     | 1'-10" | 7"     | 1'-10" | -     | - | 0  | - | - | - | -        | -          | Vertical   |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          | EP409             | 28  | 4'-2"  | S    | 0     | 1'-10" | 6"     | 1'-10" | -     | - | 0  | - | - | - | -        | -          | Vertical   |  |  |  |
| EP508             | 56  | 5'-3"  | Horizontal          |  |     |        |          |      |     |        |          | EP410             | 14  | 4'-6"  | S    | 0     | 1'-10" | 10"    | 1'-10" | -     | - | 0  | - | - | - | Vertical |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |
|                   |     |        |                     |  |     |        |          |      |     |        |          |                   |     |        |      |       |        |        |        |       |   |    |   |   |   |          |            |            |  |  |  |

Note: Quantities are given for a total of 14 End Posts - 4 each for I-95 over MCRR, I-395 and Stillwater Ave., and 2 for I-95 over Broadway

| FWHA  | STATE | PROJECT NUMBER | SHEET NO. | TOTAL SHEETS |
|-------|-------|----------------|-----------|--------------|
| MAINE | MAINE | R-95-8(130)175 | 26        | 34           |



All dimensions are out to out of reinf. bar  
 Bending details and hooks shall conform to the recommendations of ACI Standard 315-65.  
 Reinforcing Bar: ASTM A615 Grade 60

- GENERAL NOTES
- First digit(s) following the letter of the Mark indicates size of reinf. bar.  
 Mark (A502) bar size - #5  
 Mark (P1001) bar size - #10  
 Mark (S603) bar size - #6
  - Letter of Marks A, P & S locates bars of Abutments, Piers, and Superstructure parts respectively.

STATE OF MAINE  
 DEPARTMENT OF TRANSPORTATION  
 INTERSTATE 95 NORTHBOUND OVER  
 STILLWATER AVENUE  
 BROADWAY  
 INTERSTATE 395  
 MCRR AND PERRY ROAD  
 BANGOR  
 REINFORCING STEEL SCHEDULE  
 SHEET 9 OF 12 AUGUSTA, MAINE  
 R-95-8(130) BANGOR

R90-168

|        |           |                 |           |               |
|--------|-----------|-----------------|-----------|---------------|
| DATE   | BY        | DESIGN - DETAIL | CHECKED   | FIELD CHANGES |
| Apr 83 | W. Cannon | W. Cannon       | W. Cannon | W. Cannon     |
| 5/8/83 | W. Cannon | W. Cannon       | W. Cannon | W. Cannon     |